

# Alley, Loading Berth & Solid Waste Standards



**City of Seattle**

July 11, 2019

# SOLID WASTE STANDARDS



# RESEARCH FINDINGS

- Low recycle/compost rate for multifamily developments
- Need to increase to meet City’s goals
- Found proximity of containers increases success



MULTIFAMILY RESIDENTIAL RECYCLING RATE

COMMERCIAL RECYCLING RATE

SINGLE-FAMILY RESIDENTIAL RECYCLING RATE



2022 MF Recycling Goal of 54%



2017 MF Recycling Rate = 36.8%

Down 1.8% from 2016



2022 Commercial Recycling Goal of 75%



2017 Commercial Recycling Rate = 65%

Up 1% from 2016



2022 SF Recycling Goal of 83%



Recycled an additional 3,662 tons in 2017



2017 SF Recycling Rate = 73.5%  
Down 0.4% from 2016

# OMNIBUS BILL (SOLID WASTE ITEMS)

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Two proposals part of SDCI Omnibus Bill

- Increase overhead clearance from 21 feet to 24 feet
  - Only required for planned overhead lift
  - Needed to accommodate actual space needed
  - Trucks have always needed 24 foot clearance
- Max 6% slope for waste management haulers access
  - Current language only for access to smaller dumpsters
  - Needed regardless of the dumpster size

Timeline

- Introduced June 2019
- Expected Adoption July/August 2019



# SOLID WASTE STANDARDS

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Proposals to amend current Land Use Code requirements

- Collection on every floor of multifamily/mixed use building
  - Idea: 3+ stories
  - Options: chutes, containers on each floor, or combination
- Updating exception criteria to provide clear guidance
- SPU review of storage room size deductions
- Other smaller changes





# ALLEY AND LOADING BERTH STANDARDS

# WHAT IS AN OFF-STREET LOADING BERTH?

- Parking space designed for delivery vehicles
- Located on private property (out of right-of-way system)
- Usually geared toward back-of-house functions
- Many different layouts



# BEGINNING OF LOADING RESEARCH

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## Response to 2018 Council Statement of Legislative Intent

- Focused on Alley Congestion in Downtown
- Recommended solution topics included:
  - Policy
  - Operations
  - Design
  - Enforcement
- Design Findings- “Examine standards for adequate loading facilities for future developments.”





# DOWNTOWN SPECIFIC RESEARCH

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## 2018 Alley Infrastructure Inventory Study

- Looked at Alley Conditions in Center City\*
- 417 Alleys in Center City\*
- 90% are one-way
- 73% of alleys are used for parking access
- Study encourages use and development of load/unload areas in new buildings



Map by UW Urban Freight Lab

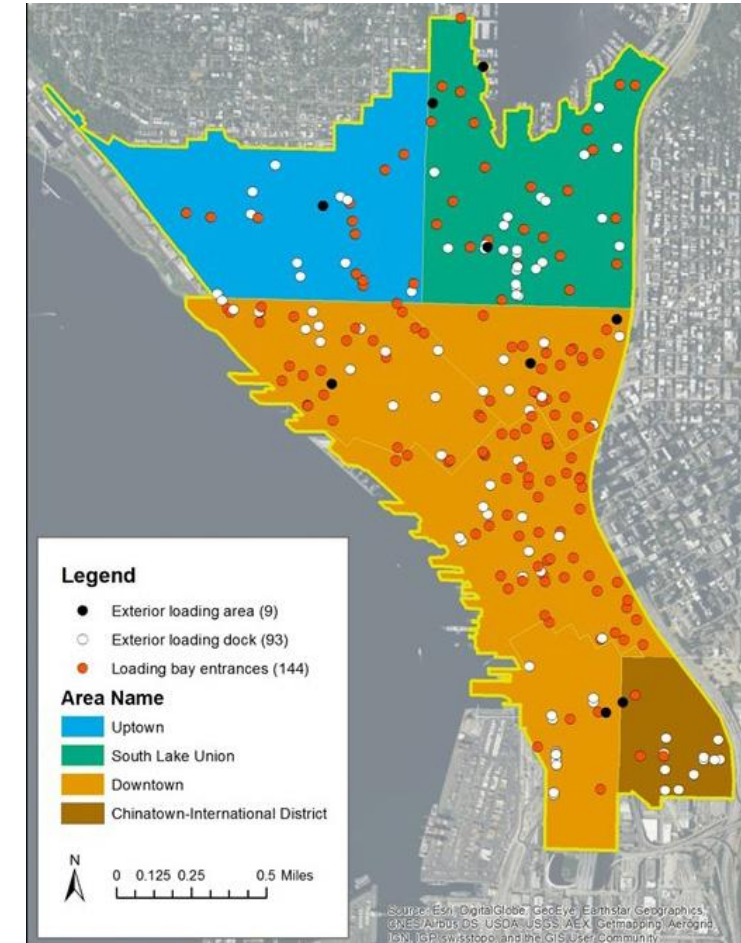
\*Downtown, Uptown, South Lake Union, Capitol Hill and First Hill

# OTHER DOWNTOWN RESEARCH

## The Final 50 Feet

- Goal is to reduce stoppage time and failed delivery attempts
- Growth of e-commerce deliveries should double good deliveries by 2023
- 87% of Buildings in Urban Core\* rely on on-street delivery space
- Recommends design and functional solutions

\*Downtown, Uptown and South Lake Union Urban Centers



Map by UW Urban Freight Lab

# RESEARCH FINDINGS

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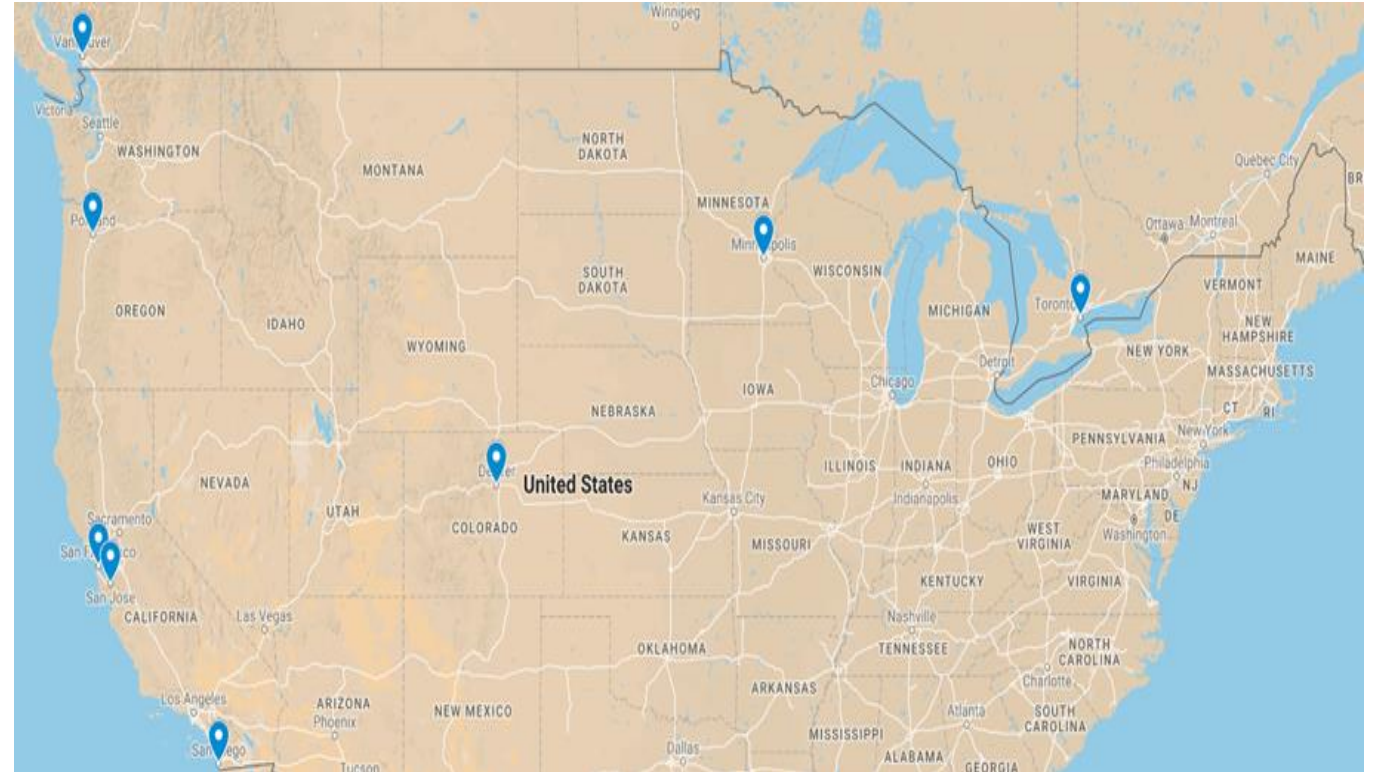
- Population Up, Delivery Demand Up, Traffic Up
- On-street loading zones - compete with many other uses
- Alley network- needs to maintain functionality
- Existing loading berth standards are outdated
- Effects of no residential loading standards



# RESEARCH FINDINGS – OTHER CITIES

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- Researched 8 peer cities
- Standards vary between cities
  - Number of Spaces
  - Demand Use Categories
  - Zones where required
  - Exemptions/Waivers
- Some similarities in sizes of loading berths



# RESEARCH FINDINGS – OTHER CITIES

- Seattle’s code only accounts for berths by use type and size of building
- Several others account for variety of delivery vehicles in standards



NAME	EXAMPLES AND DATA-COLLECTION NOTES
TRUCK OR VAN TYPES  Truck (T)	 <p><i>Truck with trailer, 3 or more axels</i></p>
Box Truck (B)	 <p><i>Single-unit trucks, 3 axels or less</i></p>
Garbage Truck (G)	
Cargo Van (CV)	
Service Van (SV)	
Van (V)	 <p><i>A cargo or service van usually displays a business logo. If there was not enough information visible, data collectors marked the vehicle as a van.</i></p>

# RESEARCH FINDINGS – OTHER CITIES

## Is Off-Street Residential Loading Required Somewhere in the Code?

Seattle	Denver	Minneapolis	Portland	San Diego	San Fran	San Jose	Toronto	Van. Can
No	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes

- All have residential loading standards
- Out of 25 largest US cities\*
  - 17 require residential loading
  - Three sometimes do
  - Five do not

\*Not including Seattle

City	Res Loading
New York	No
Los Angeles	Yes
Chicago	Yes
Houston	Yes
Philadelphia	Yes
Phoenix	Yes
San Antonio	Yes
San Diego	Yes
Dallas	No
San Jose	Yes
Austin	Yes
Jacksonville	Yes
San Francisco	Yes
Indianapolis	Yes
Columbus	No
Fort Worth	Sometimes
Charlotte	Yes
Seattle	No
Denver	Yes
El Paso	No
Detroit	Yes
Washington	Yes
Boston	No
Memphis	Maybe
Nashville	Maybe
Portland	Yes

# RESEARCH FINDINGS – SCENARIO

Seattle’s current code requires more spaces than others

- Large downtown residential building with street-level retail
- Residential – 636 units (783,000 gsf)
- Retail – 35,000 gsf

## Number of Spaces Required for Scenario

Seattle	Denver	Minneapolis	Portland	San Diego	San Fran	San Jose	Toronto	Van. Can
8*	5	3 small or (2) 1 small & 1 large	2 Big or (3) 1 big and 2 small	5	Max is 4	5	(4) 3 Medium and 1 Small	(7) 6 medium and 1 large

\*If Seattle’s existing low demand requirement is applied to residential loading

# AMENDMENT GOALS

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## Loading Berth and Alley Standards

- Meet increased delivery demand in new buildings
- Preserve functionality of alley and street networks
- Improve freight and delivery systems



## Solid Waste Standards

- Ensure new buildings account for solid waste needs





# TWO MODES FOR CHANGES

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## Director's Rule

- Interprets Existing Code Requirements
- Reviewed by Department
- Approved by SDCI Director
- Short-Term Solutions
- Minimum Time- 3 Month Process

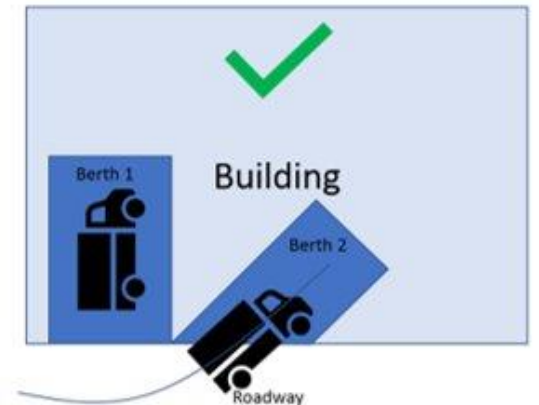
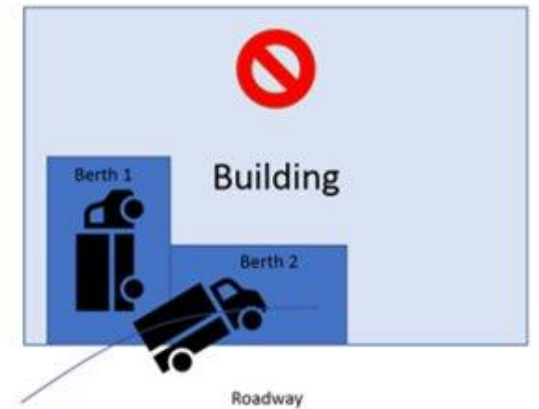
## Code Amendment

- Approves New Code Requirements
- Reviewed/Released by Mayor's Office
- Approved by City Council
- Long-Term Solutions
- Minimum Time- 1 Year Process

# DIRECTOR'S RULE

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- Interprets Existing Code
- Residential Standards
  - Initial Proposal - 1 berth for any building over 40,000 gsf\*
- Maneuverability Analysis for Loading Berths
  - Limit the number of maneuvers
  - Berths independently accessed
  - Loading berths cannot extend into the alley or setback
  - 3-D space of berth remains unencumbered
- Solid Waste Placement
  - Show solid waste storage, staging, and collection on site plan
  - Staging and collection in ROW would require approval by SDOT and SPU

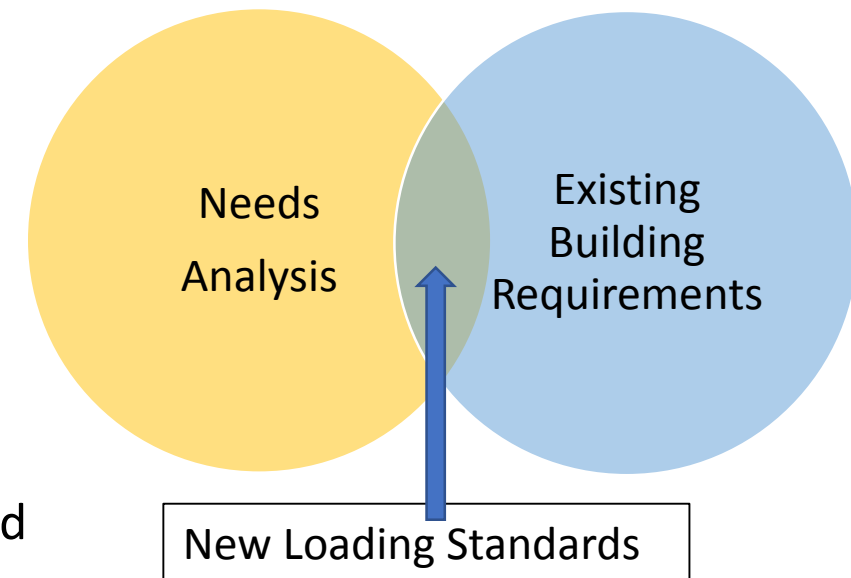
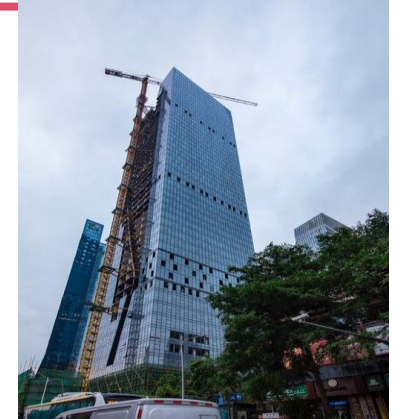


\*Specific standard may change as a result of outreach feedback

# ALLEY/LOADING CODE STANDARDS

## Proposals to Amend Current Land Use Code Requirements

- Develop a Needs Analysis
  - By Use Type
  - By Building Size
  - Sizes of loading berths within buildings
  - This will include creating a residential standard
- Accounting for other ground-floor functions
  - Many other needs on the ground floor
  - Functional needs
  - Aesthetic and street-frontage needs
- Updating Exception Criteria
  - Based on objective criteria
  - Citywide exceptions (currently limited to certain areas)
  - Require Loading Operations Plans when exceptions requested



# ALLEY/LOADING STANDARDS

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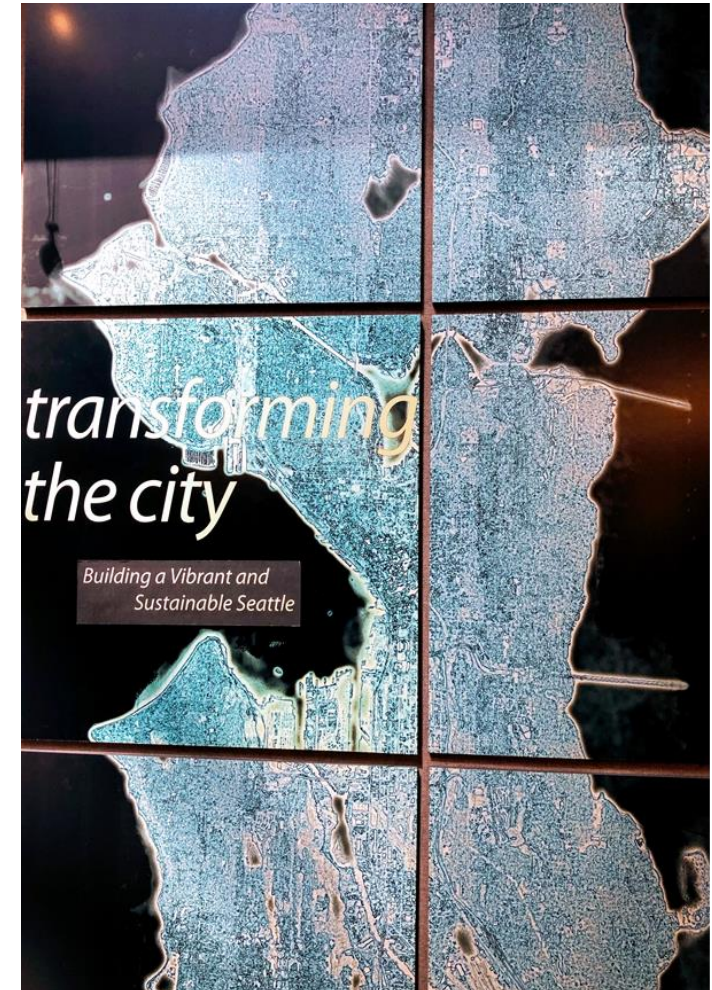
## Other Possible Changes

- Limit Design Review Board ability to modify loading berths
- Allow shared solid waste storage between buildings
- Allow shared loading berths between buildings
- Alleys need concrete or asphalt for solid waste access and collection
- Update vehicle access (entrance location) hierarchy for consistency across zones
- Require on-site solid waste collection for larger projects

# NEXT STEPS FOR POTENTIAL CHANGES

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- More focus group meetings and outreach
- Create webpage with background information
- Continue research and feedback
- Provide updates
- Prepare draft recommendations



# STAY INVOLVED

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